

**City of Baldwin Park
2020 General Plan**

Urban Design Element

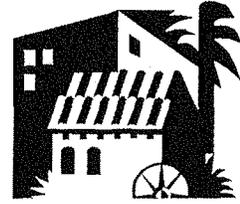
November, 2002

Table of Contents

Introduction	UD-1
Purpose of the Element	UD-1
Element Scope and Content	UD-1
Related Plans and Programs	UD-1
Issues, Goals and Policies	UD-3
Gateway Entrances	UD-3
I-10 San Bernardino Freeway Corridor	UD-4
Downtown	UD-4
Aesthetic Quality of Development	UD-6
Streetscape Environments	UD-7
Residential Neighborhoods	UD-7
Urban Design Plan	UD-9
City Gateways	UD-9
I-10 Freeway Corridor	UD-11
Streetscape Improvements	UD-12
Downtown	UD-14
Residential Neighborhoods	UD-18

List of Figures

UD-1	Urban Design Plan	UD-10
UD-2	City Gateway Signage	UD-11
UD-3	Main Avenue Street Improvements	UD-13
UD-4	Downtown Pedestrian Improvements	UD-15
UD-5	Mid-Block Crossing	UD-16
UD-6	Paseo	UD-16
UD-7	Metrolink Pedestrian Bridge	UD-17
UD-8	Metrolink Pedestrian/Building Linkage	UD-17
UD-9	Hedge Fence	UD-18
UD-10	Picket Fence	UD-19
UD-11	Wall Treatment	UD-19



URBAN DESIGN ELEMENT

Introduction

Purpose of the Element

The Urban Design Element addresses the City's visual and aesthetic resources, focusing on the quality and character of public areas and private development in Baldwin Park. It builds upon the Land Use Element.

The goals of the Urban Design Element can be attained with a reasonable level of resources. The City's built-out condition does not require aggressive urban design plans to change the basic nature of the City's existing districts. The challenge is to work carefully with existing assets and provide an overall visual structure that builds a city identity, links its neighborhoods, and increases awareness of its assets and resources.

Element Scope and Content

The Urban Design Element defines Downtown and other major districts, neighborhoods, freeway corridors, boulevards, and open spaces with distinctive appearances and qualities. Goals and policies are referenced to the Land Use Element and other components of the General Plan.

The first section of the Element identifies issues, goals and policies that support important urban design improvements for Baldwin Park. The second section of the Element, The Urban Design Plan, presents urban design concepts and recommendations related to specific parts of the City.

Related Plans and Programs

Design guidelines for specific types of development supplement the policy recommendations of the Urban Design Element. Published as separate documents, they are to be used by developers, property owners, and design professionals in the planning of new construction or major renovations in the City. They are also to be used by City staff, Commissions, and the City Council as adopted criteria for the review of development proposals subject to discretionary design review.

These guidelines include:

- *Design Guidelines for Single-Family Detached Planned Residential Development*
- *Design Guidelines for Single-Family Detached Residential Tract Development*
- *Design Guidelines for Commercial and Industrial Development within the Sierra Vista Redevelopment Project Area*

These guidelines may be amended from time to time as needed.

Issues, Goals and Policies

The following six major issues are addressed by the goals, policies and implementation actions of the Urban Design element: (1) provide urban design opportunities for gateway entrances in Baldwin Park; (2) establish a positive image on the I-10 San Bernardino Freeway corridor; (3) establish Downtown as a pedestrian-oriented zone; (4) enhance the aesthetic quality of development in the City; (5) improve streetscape environments to enhance the city's environmental conditions; and (6) enhance residential neighborhoods in Baldwin Park.

Gateway Entrances

A "gateway" is a city access or point of entry. The two primary gateways are located on Ramona Boulevard just east of I-605 and on the east side at Badillo Street. Improvement of established and creation of new gateways will begin to create a sense of identity and place for Baldwin Park within the San Gabriel Valley.

Goal 1.0 Improve City gateway entrances to welcome visitors and enhance the city image at primary and secondary entrances to the City.

Policy 1.1 Provide new City monument signs at primary gateways:

- Ramona Boulevard & 605 Freeway gateway
- Los Angeles Street gateway (west side)
- Pacific Avenue gateway
- Baldwin Park Boulevard (south City limits)
- Maine Avenue at Arrow Highway
- Arrow Highway (east City limits)

Policy 1.2 Provide new City entry signage at secondary gateways:

- Francisquito Avenue (south City limits)
- Puente Avenue (south City limits)

Policy 1.3 Establish a consistent design style for all public signage, including fixture type, lettering colors, symbols, and logos.

I-10 San Bernardino Freeway Corridor

The I-10 San Bernardino freeway corridor extends through the south side of the City between the I-605 and Baldwin Park Boulevard. Due to its location and high volume of commuters, it is a “regional window” where Baldwin Park’s image is perceived. The issues identified in this corridor include lack of landscaping, need for screening of undesirable views and need for a gateway entry statement at the on-ramps. The I-10 corridor offers a great opportunity to present a positive City image.

Goal 2.0 Establish a positive City image on the I-10 Corridor between I-605 and Baldwin Park Town Center (Merced Avenue). Create a “green corridor” on both sides of the freeway.

Policy 2.1 Provide coordinated landscape design within the corridor.

- Coordinate with Caltrans during the I-10 widening and access improvements for landscape enhancements, soundwall design and materials, signage and access.
- Plant clinging vines to all freeway abutments, grade separations and pedestrian bridges to enhance the green corridor concept and discourage graffiti.

Policy 2.2 Establish landscape buffers and/or aesthetically treated walls to screen unsightly views.

Policy 2.3 Provide special landscaping and gateway signage and graphics at key freeway access points.

Downtown The Downtown area consists of the portion of Ramona Boulevard between Baldwin Park Boulevard and Bogart Avenue, and the section of Maine Avenue between Ramona Boulevard and Clark Street. Downtown is defined by a commercial area consisting of small retail shops on both sides of Ramona Boulevard, with off-street parking in front of the shops. Ramona Boulevard is a multi-lane traffic corridor to the I-605 freeway, and traffic often passes through the Downtown community rather than stopping at local shops.

Currently, Downtown lacks a pedestrian-friendly environment because the expansive Ramona Boulevard width tends to divide the north and south sides of Downtown. There is also a lack of direct access to the Metrolink Station, City Hall, and to Morgan Park. Downtown has all the elements to be a pedestrian-friendly environment. The challenge is to link the key Downtown elements, calm traffic, and improve the aesthetic image through design and quality materials.

Goal 3.0 Establish Downtown as a pedestrian-friendly environment.

Policy 3.1 Define Downtown as a pedestrian-overlay zone and require that new uses and buildings enhance pedestrian activity through the use of:

- Walls which are aesthetically treated by the use of color, textured materials, offset planes, recessed entries and windows, columns, and other architectural details to provide visual interest to pedestrians
- Extensive landscaping, including trees, flowering shrubs and groundcover
- Pedestrian amenities, such as street furniture, trash receptacles and signage oriented to the pedestrian
- Design amenities related to the street-level, such as awnings, colonnades, arcades and paseos
- Visual access to the interior of buildings

Policy 3.2 Develop a streetscape program, including signage, undergrounding of utility lines, and street tree planting, to improve the appearance of Maine Avenue. Establish a district gateway entrance into Downtown by extending the Maine Avenue streetscape concept from Downtown north to the Performing Arts Center.

Policy 3.3 Examine potential transit opportunities which could be developed to link this area with the Metrolink station.

Policy 3.4 Establish a pedestrian linkage between the Metrolink Station and Downtown.

- Connect to Downtown by establishing a street promenade from Railroad Avenue to Ramona Boulevard
- Enhance the west pedestrian connection on Downing Avenue to City Hall and Downtown
- Evaluate the establishment of a pedestrian crossing over the rail right-of-way to connect the Metrolink Station to Downtown. Alternatives to consider include: at grade crossing, pedestrian bridge and second level pedestrian way connecting a building.

Policy 3.5 Create an active "paseo" in the courtyard of City Hall to connect to Downtown. Continue the colonnade concept on Maine Avenue to the south side of Ramona Boulevard connecting to the City Hall courtyard. Provide a paseo between the City Hall courtyard to the street promenade, and enhance with landscaping, paving and lighting.

Policy 3.6 Establish a mid-block pedestrian connection between the north and south side of Ramona Boulevard with a pedestrian activated signal. Utilize traffic calming techniques such as enhanced paving within the crossing to provide a visual and auditory change in surfaces.

Policy 3.7 Extend the Maine Avenue streetscape south on Pacific Avenue to the Dalton Wash.

Policy 3.8 Establish public graphics and signage for Downtown.

- Develop Downtown logo
- Develop public graphics and signage program for Downtown

Policy 3.9 Consider Downtown as a museum without walls by creating areas for public art such as murals, sculptures, and fountains.

Policy 3.10 Complete streetscape improvements on the south side of Ramona Boulevard.

- Provide street trees, street furniture and aesthetic wall and fencing design for the Corridor. Coordinate landscaping with the new sequence of land use patterns.
- Complete undergrounding of the utility lines on Ramona Boulevard.

Aesthetic Quality of Development

The quality of development in Baldwin Park depends upon the implementation of good design based upon adopted development standards and guidelines. These standards and guidelines are to be evaluated for their effectiveness and accomplishment of their stated objectives.

Goal 4.0 Enhance the aesthetic quality of development in the City.

Policy 4.1 Require that signage on commercial structures be compatible and integrated within the structure's architecture.

Policy 4.2 Review and revise, as necessary, the City's development standards to improve the quality of new development and to protect public health and safety.

Streetscape Environments

The City's arterial streetscapes form the visual frame of the community. These major streetscapes are the paths of movement from which most residents and visitors experience the community. Streetscape improvement issues include overhead power lines, lack of street trees, and lack of consistent street furniture, lighting and signage. The community's image can be readily enhanced and reinforced by the repetition of these distinctive streetscape elements.

Goal 5.0 Promote urban forestry concepts to enhance the environmental conditions in the city by providing shade, shelter from inclement weather, improved air quality, and aesthetic improvements to neighborhoods.

- Policy 5.1 Develop a city-wide master street tree plan.
- Policy 5.2 Coordinate street tree planting with street widening and sidewalk improvement projects.
- Policy 5.3 Maintain and conserve existing street trees, and require replacement where trees are removed.
- Policy 5.4 Establish street tree management plan and policies to monitor, maintain and replace the City's street trees.
- Policy 5.5 Encourage residents to be active in taking care of their parkway trees. Provide educational programs and materials in the care of trees and concepts of urban forestry.
- Policy 5.6 Establish a priority plan and program to underground utility lines at key areas in the City to provide tree planting opportunities.

Residential Neighborhoods

The chain link and wrought iron fences found in many of the neighborhoods exhibit an unfriendly neighborhood image and imply security problems. Proper street tree planting in keeping with the scale and image of a safe, clean neighborhood is needed. Baldwin Park's neighborhoods can be enhanced to create a better aesthetic environment.

Goal 6.0 Enhance the neighborhood environments within Baldwin Park.

- Policy 6.1 Provide guidelines for aesthetically pleasing front yard fencing and fence hedge plantings to minimize the utility chain-link gated look.
- Policy 6.2 Buffer residential from non-residential uses with aesthetically pleasing walls, landscaping and/or fencing.

Policy 6.3 Provide educational programs and materials for residential landscaping and water conservation methods. Provide plant list suitable for Baldwin Park's horticultural environment.

Policy 6.4 Within residential streets, select street trees which will provide substantial amount of shade and provide canopies over the street at maturity.

Urban Design Plan

The Urban Design Plan addresses improving the quality and visual images of the City. The Urban Design Plan is an overall master plan that develops a design framework to guide future improvements and redevelopment of the City's districts and neighborhoods to insure continuity and proper execution of the plan. The Urban Design Plan will provide guidance for decision makers and staff, ensuring continuity of execution over time.

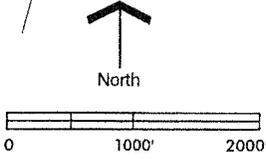
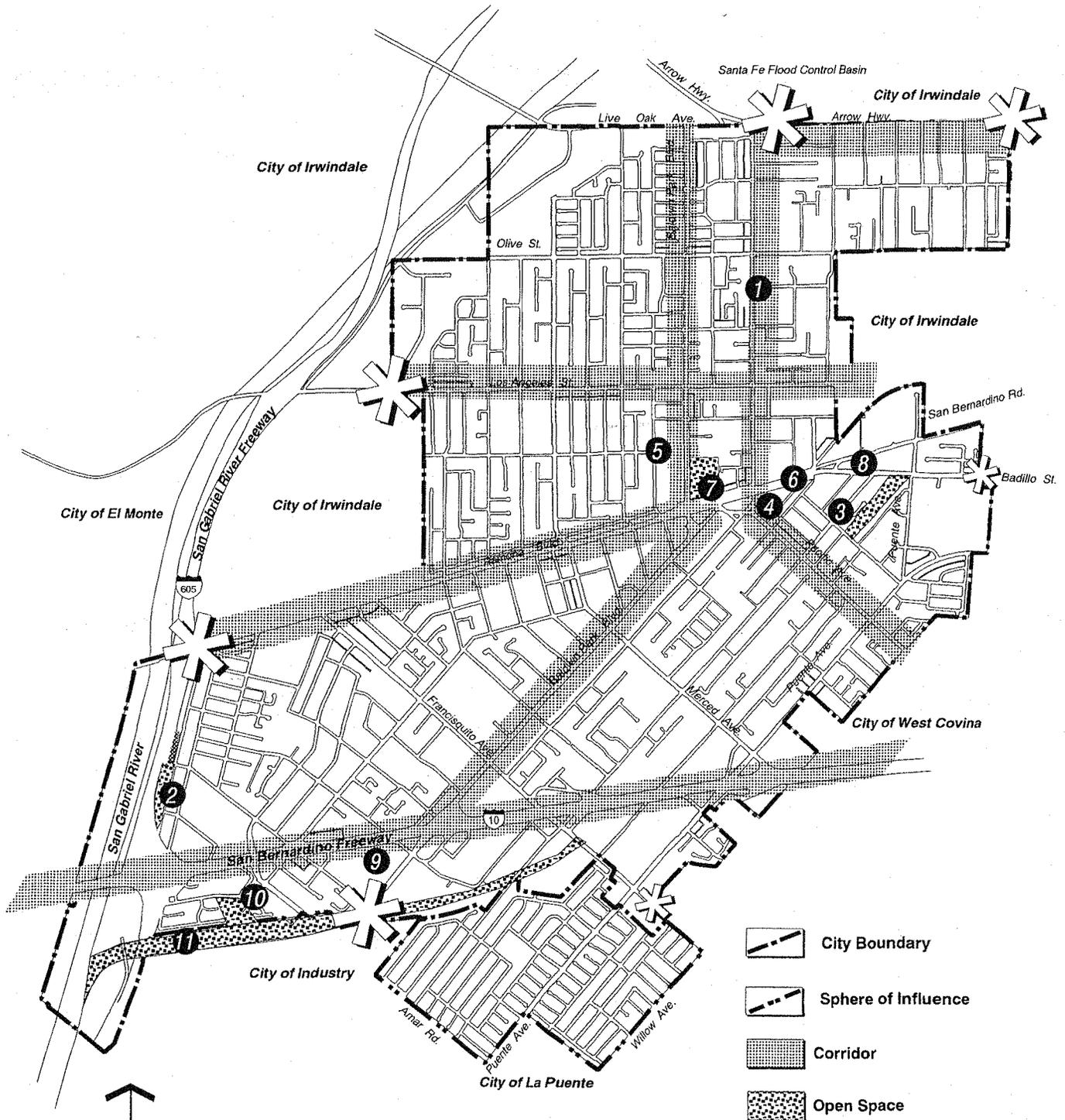
Although Baldwin Park is largely a built-out community, the City has the opportunity to improve its overall image by capitalizing on the quality of its best sections, improving its public rights-of-way, and enhancing development standards for new construction and redevelopment. The key features of the Urban Design Plan are illustrated in Figure UD-1 and are discussed as follows. They include:

- City Gateway Entrances
- I-10 San Bernardino Freeway Corridor
- Streetscape Improvements
- Downtown
- Neighborhood Improvements

City Gateways

Entrances to the City should be marked by distinctive design features that clearly communicate the community's commitment to high quality design and development.

As illustrated by Figure UD-2, the gateways are preferably located on a raised median island and/or in parkways on both sides of the street. Some form of substantial monumentation bearing graphics to reinforce City identity, accompanied by complementary landscape plantings and accent lighting, should welcome visitors and residents alike. Specimen trees and annual color can often be effectively incorporated in gateway monumentation. These gateways are recommended at primary and secondary entrances.



- 1 Performing Arts Center and Community Adult School
- 2 Barnes Park
- 3 Central Park West
- 4 City Hall and Old School Auditorium
- 5 Library
- 6 Metrolink Station
- 7 Morgan Park/Community Center
- 8 Old Electric Power Station
- 9 Road Side Park
- 10 Walnut Creek Nature Park
- 11 Walnut Creek Wash

- City Boundary
- Sphere of Influence
- Corridor
- Open Space
- Primary City Gateway
- Secondary City Gateway

**Figure UD-1
Urban Design Plan**

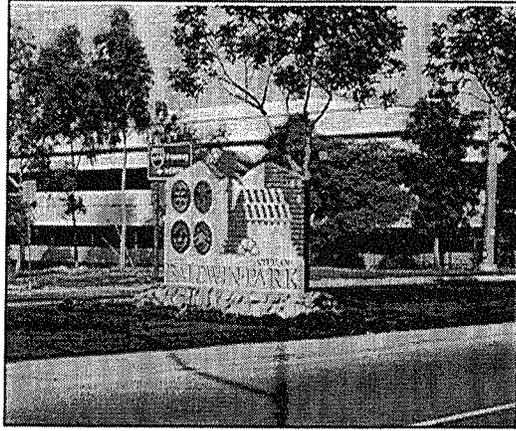


Figure UD-2
City Gateway Signage

Entrances to the community should be created at each of the following primary gateways, as illustrated in Figure UD-1.

- Ramona Boulevard and I-605 Freeway gateway
- Los Angeles Street gateway (west side)
- Pacific Avenue gateway
- Baldwin Park Boulevard (south City limits)
- Maine Avenue at Arrow Highway (north City limits)
- Arrow Highway (east City limits)

Secondary gateways should be recognizable by appropriate monumentation and plantings while still reflecting the primary gateway theme. At a minimum, new entry signage should be provided at the following secondary gateways:

- Francisquito Avenue (south City limits)
- Puente Avenue (south City limits)
- Badillo Avenue (east City limits, existing)

I-10 Freeway Corridor

The I-10 Freeway, traversing the City along its southern edge, offers windows to the community and a unique opportunity to enhance Baldwin Park's image within the region. Freeway access and points of entry at major arterials offer opportunities to create well-designed municipal gateways.

The photo simulation below illustrates the concept by enhancing both sides of the I-10 Freeway with a well landscaped corridor within the Caltrans and City right-of-way, creating a colorful and green passage through Baldwin Park. Objectionable views should be screened from public view, and freeway soundwalls should be planted with clinging vines to soften the wall surface and discourage graffiti. When new development occurs along the corridor, developers should contribute and participate in the enhancement of the freeway corridor.



Landscape Enhancement to I-10 Freeway Corridor

Streetscape Improvements

Key arterials form the visual frame of the City, leading from points east, west, north and south to the heart of the community in Downtown. These major streets are the paths of movement from which most residents and visitors experience the community. These streets are given high priority as elements for upgrading City image. Baldwin Park Boulevard, Ramona Boulevard, Pacific Avenue, Arrow Highway, and Maine Avenue are recommended for such treatment.

Along these key arterials, the community's image can be readily enhanced and reinforced by the repetition of distinctive streetscape elements, including:

- Street Trees
- Underground Utilities
- Street Furniture
- Enhanced Paving
- Graphics and Signage
- Lighting

Street Trees: A well-formulated street tree master plan for all major arterials and attendant management policies to monitor, maintain, replace and augment the City's street tree inventory should be prepared.

Underground Utilities: Overhead utility lines contribute to the visual clutter experienced on some of the City's arterial streets. The overhead utility lines limit tree species and pruning height. Figure UD-3 illustrates how Maine Avenue, north of Downtown can be improved with underground utilities and street tree planting.

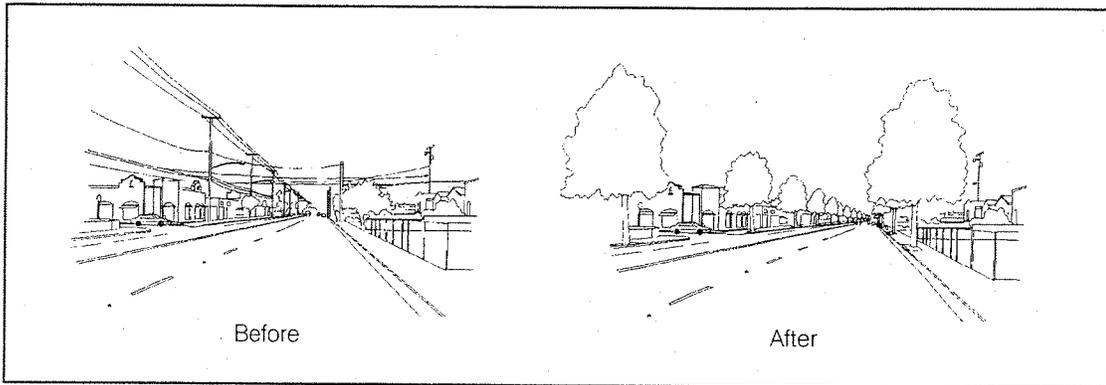
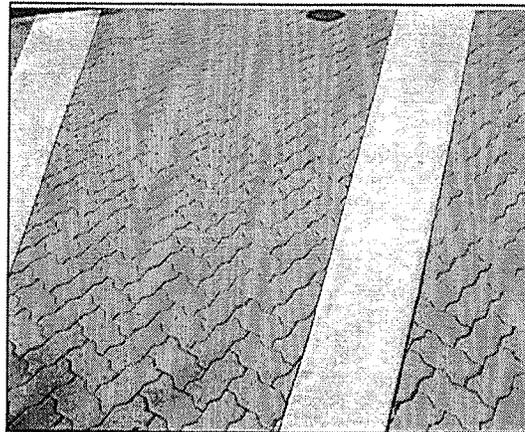


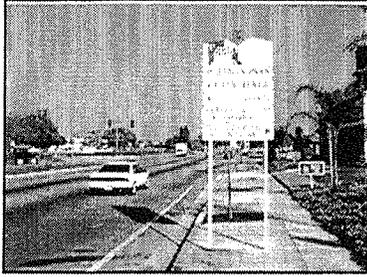
Figure UD-3 Maine Avenue Street Improvements

Street Furniture: A preferred style of street furniture and fixtures — pole masts and arms for street lighting, signalization and signage, bus benches, bollards, news racks, planters and tree grates — should be selected to typify the City.

Enhanced Paving: A distinctive enhanced paving style for all future crosswalks and median paving should be identified and specified as part of a phased program of right-of-way improvements.



Enhanced Paving



Improved Graphics and Directional Signage: A consistent graphic style for all public signage, including size, lettering, colors, symbols, and logos should be developed. Banners can also be used to reinforce the sense of entry.

Distinctive Lighting: Distinctive nighttime illumination along major arterials to be considered include accent lighting for landscaping and key landmark buildings, decorative pedestrian lighting fixtures, and the use of high-pressure sodium bulbs to create warm illumination tones.

Downtown

Baldwin Park's Downtown area generally is bounded by Ramona Boulevard from Baldwin Park Boulevard to Bogart Avenue, and Maine Avenue between Ramona Boulevard and Clark Street. A Metrolink station adjoins the Downtown core. Public streetscape improvements, building and shop front rehabilitation, high quality new infill development, improved parking and access, mixed-use housing, business promotional activities, and improved connections to public transportation all present opportunities to strengthen Downtown's attractiveness and competitive position.

To enhance the concept of a pedestrian-friendly Downtown, key Downtown elements are recommended to be visually and functionally linked in order to foster pedestrian activity between the commercial core, City Hall, Morgan Park, Post Office, future mixed use housing, the Metrolink Station and other key Downtown destinations. The pedestrian improvements are illustrated in Figure UD-4 and include the following:

- Mid-block crossing
- Paseos
- Colonnades
- Metrolink connections

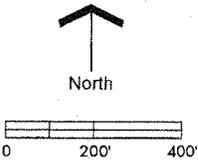
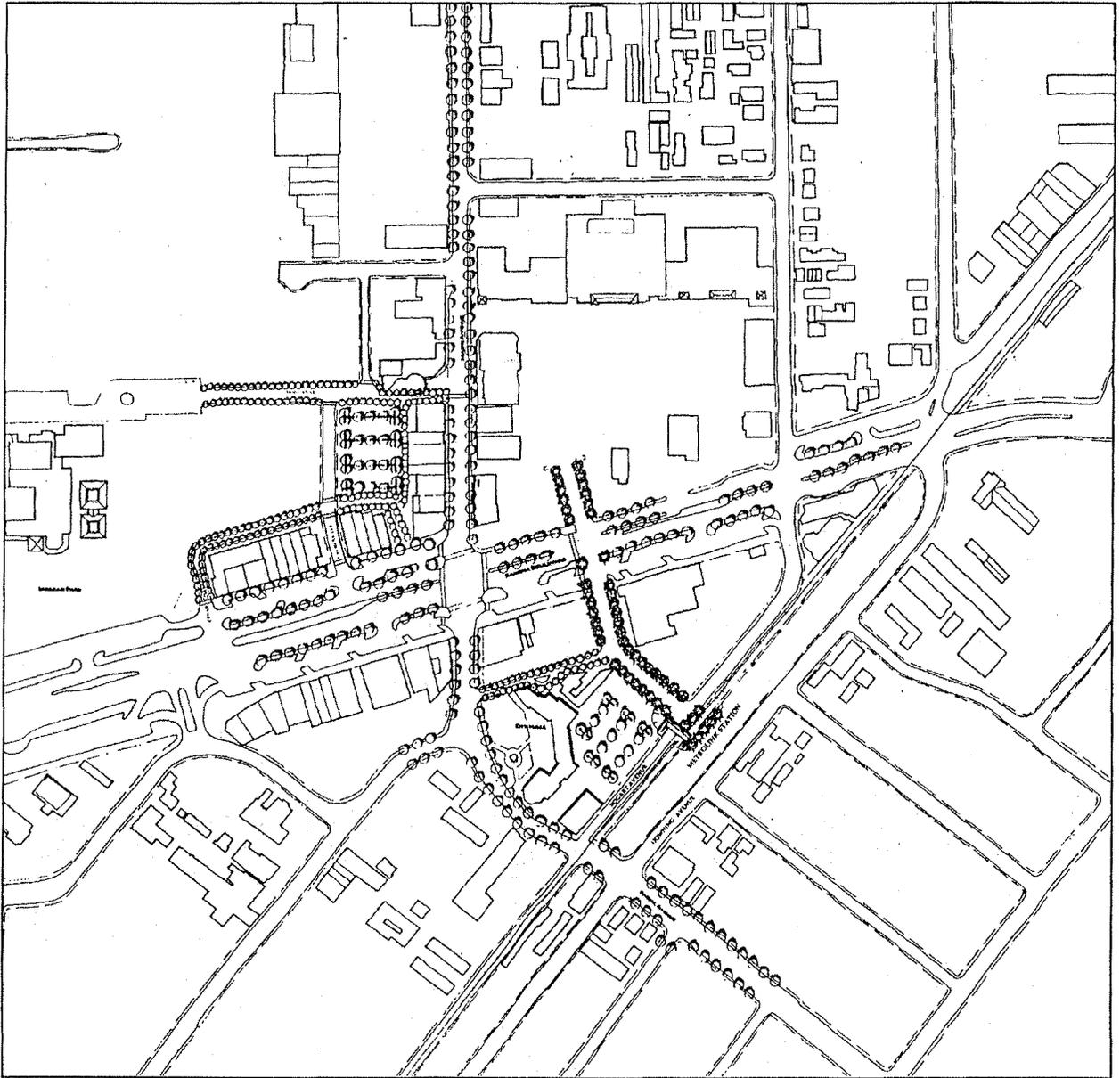


Figure UD-4
Downtown Pedestrian
Improvements

Mid-block Crossing: Figure UD-5 illustrates the Ramona Boulevard mid-block crossing which physically links the north and south sides of Downtown. The crosswalk is envisioned to provide connections between commercial areas of Downtown, future mixed-use housing, Metrolink Station, and City Hall. The crosswalk will have a traffic calming affect with its decorative and textured paving.

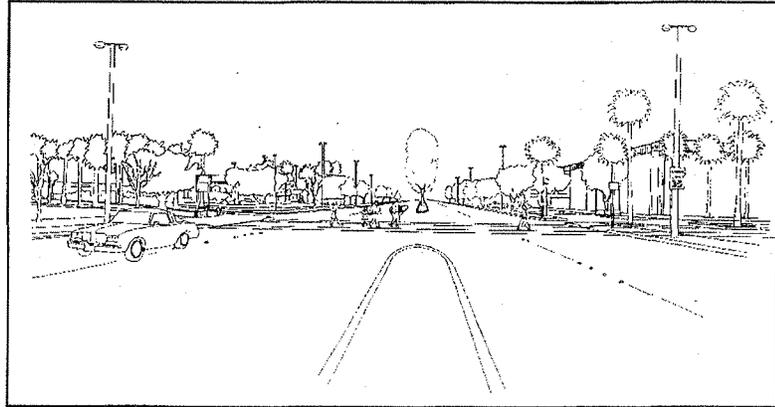


Figure UD-5
Mid-Block Crossing

Paseos: Paseos will provide pedestrian linkages to key features in Downtown. As Figure UD-6 illustrates, the paseos will provide a pleasant walking environment adjacent to the rear facades and parking areas of the commercial core. These paseos offer connections to key attractions between the commercial core, Morgan Park, senior housing, City Hall, and future mixed-use housing.

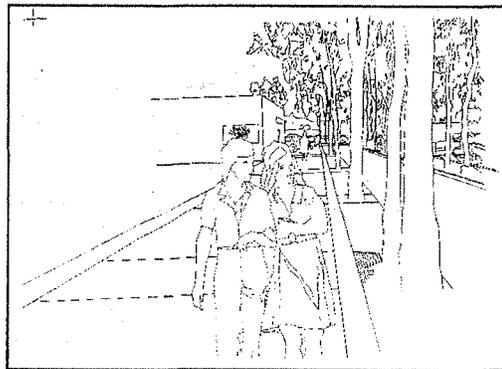
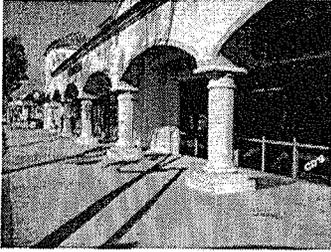


Figure UD-6
Paseo



Colonnades: Colonnades provide shade and protection during inclement weather and form outdoor rooms for dining and seating. Colonnades have been introduced into Downtown in portions of the commercial core. Providing colonnades in future developments will encourage a pedestrian-friendly environment for Downtown.

Metrolink Connections: Alternatives for a direct pedestrian connection between Downtown and the Metrolink Station should be considered, including:

- At-grade crossing between the Metrolink Station and Bogart Avenue.
- Pedestrian Bridge connection over the Metrolink tracks, as shown in Figure UD-7.
- Pedestrian bridge connection over the Metrolink tracks connecting to a future building where the elevator could be used by the disabled, as shown in Figure UD-8.

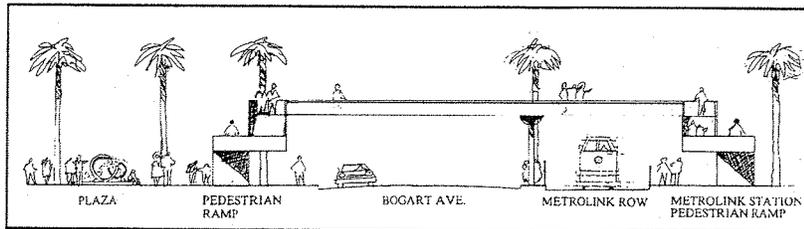


Figure UD-7 Metrolink Pedestrian Bridge

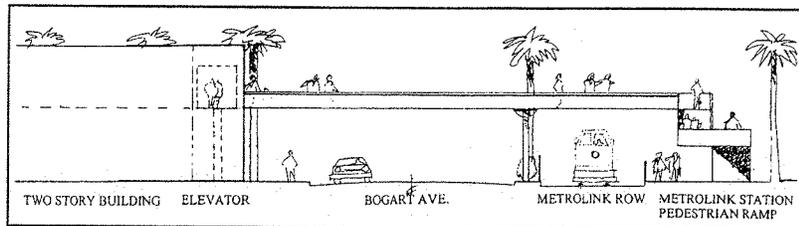


Figure UD-8 Metrolink Pedestrian/Building Linkage

Residential Neighborhoods



The residential neighborhoods can be improved with consistent solutions for street landscaping, fence, and wall designs.

Street Landscaping: The sense of neighborhood is enhanced by selecting trees which can link both sides of the street by forming a canopy with their branches. The benefits of this type of landscaping include shade, cooler temperatures, better air quality and better sense of scale.

Fence Alternatives: The chain link and wrought iron fences found in many of the neighborhoods exhibit an unfriendly neighborhood image and suggest security problems. The following fence alternatives are recommended to enhance the neighborhood environment:

Landscape Hedge: Figure UD-9 illustrates the planting of a hedge adjacent to the chain link or wrought iron fence. The hedge will eventually grow through the fence opening, forming a green perimeter.

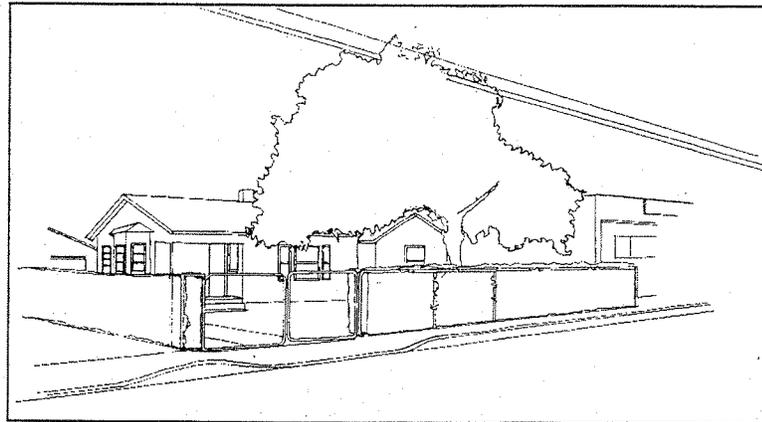


Figure UD-9
Hedge Fence

Picket Fence: Figure UD-10 illustrates the picket fence alternative. The white picket fence portrays a friendlier image and can also be enhance with landscaping planted behind it. The picket fence may be constructed of wood or modern vinyl product, which is more resilient and requires less maintenance.

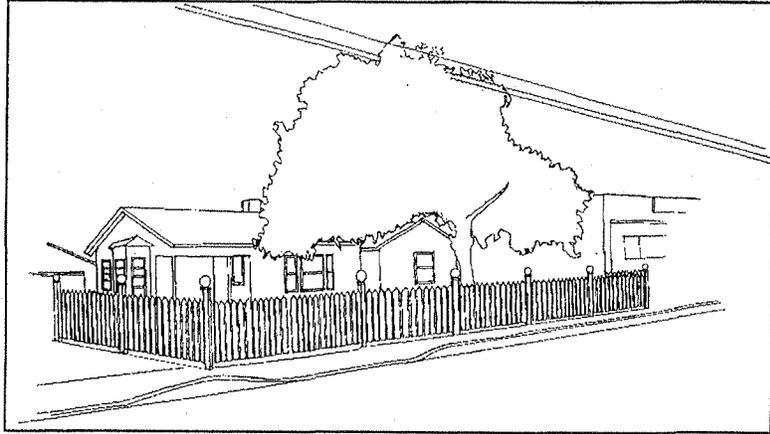


Figure UD-10
Picket Fence

Wall Design: The following wall treatments should be considered to minimize the mass and scale of walls and graffiti. Figure UD-11 illustrates the use of pilasters and ornamentation to reduce wall expanse. These design enhancements provide interest to the wall and shadows which tend to minimize the mass. All walls should have a graffiti coating for easy removal. Additional techniques to consider include clinging vines and split-face and concrete block.

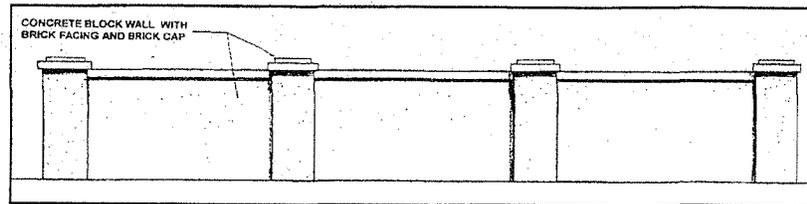


Figure UD-11
Wall Treatment