

CHAPTER 5 INDUSTRIAL



I. INTRODUCTION

A. Purpose

This chapter provides guidelines for designing new industrial developments and for exterior alterations and additions to existing developments. Because of the size and scale of industrial buildings, it is especially important to consider design to ensure compatibility with other parts of the community.

As a category of structure types, industrial buildings often present unattractive and monotonous facades with large blank wall surfaces, untreated or false fronts, or highly reflective and glaring surfaces. In addition, the site development is often not pedestrian-friendly, not properly buffered from surrounding uses, insufficiently landscaped, and surrounded by unsightly fencing. There are, however, a variety of design techniques which can be utilized to help overcome these situations and to direct development into a cohesive design statement that is both functional and aesthetically appealing.

Property owners, developers, architects, building designers, and contractors seeking to construct new industrial developments, or alterations or additions to existing developments, should use these guidelines in the early design stages of their projects. These guidelines are not intended to limit creative site planning and architecture that are consistent with the stated goals and within the context of surrounding neighborhood patterns. Innovative design solutions are strongly encouraged.

Refer to the City of Baldwin Park Zoning Code for specific development standards pertaining to industrial uses.

B. Applicability

These guidelines apply to all industrial development and business parks, including smaller infill projects as well as larger master planned sites, and are in addition to the development standards set forth in the Zoning Code.

C. Administration

The following projects are subject to Design Review by the Design Review Committee in accordance with Chapter 153.210 of the Zoning Code:

- Construction of a new industrial building or development.
- Addition or exterior alteration to an existing industrial building or development.

Evaluation of Design Review shall be based on substantial compliance with the intent and guidelines set forth in this chapter, and projects shall be approved, conditionally approved or denied on such basis.

D. Design Guideline Goals

The guidelines have been established in order to accomplish the following goals:

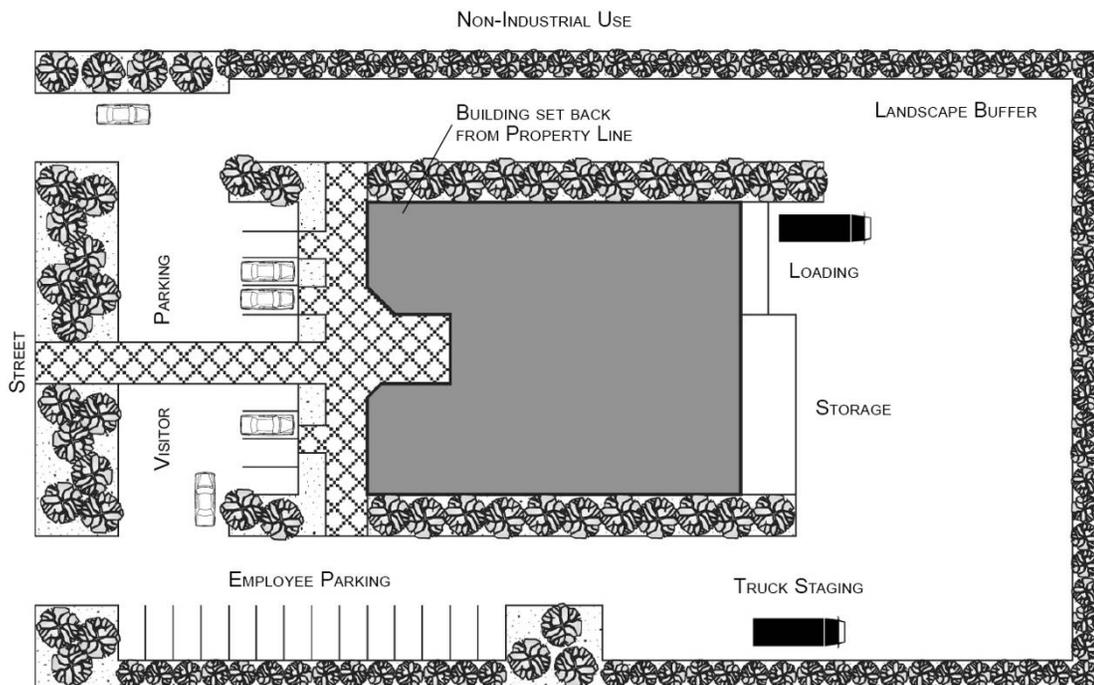
- Improve the quality of design for industrial developments, thereby improving the image and appearance of the City's industrial areas.
- Create attractive and functional site arrangements of buildings, service and loading areas, open spaces, and parking areas; and develop a high quality architectural and landscape design.
- Contribute to the character of the neighborhood by respecting the scale, proportion and architectural style of the surrounding area.
- Create visual interest in industrial buildings, while maintaining a sense of harmony within the project.
- Mitigate the negative impacts and views associated with industrial uses through effective site placement, screening, and buffering techniques.
- Eliminate random development patterns and establish site planning and design relationships between new development and neighboring properties.
- Encourage environmental sensitivity in development.
- Improve pedestrian circulation and connections on industrial sites and within industrial areas.
- Improve the appearance and character of the freeway corridor.

II. SITE DESIGN GUIDELINES

Industrial site design must be functional and efficient, as well as compatible with adjacent land uses and aesthetically appealing. Elements of sound industrial site design include emphasis on the main building entry and landscaping; provision of pedestrian walkways and connections; plazas and landscaped open space areas for employees; convenient and controlled access, visitor parking and on-site circulation; screening of outdoor storage and loading areas; and appropriate buffering between incompatible land uses. Also, it is important to consider a project's relationship to adjacent industrial properties in creating a unified development pattern for the surrounding area.

A. Building Orientation, Siting and Entrances

1. Buildings in an industrial development should be arranged to create a sense of unity and overall harmony. Avoid random and irregular building relationships.
2. Site development, including location of building, parking, and landscape areas, should consider compatible development patterns among neighboring properties. In addition, consideration should be given to how future neighboring developments, based on existing lot patterns, could relate to the project. As far as is feasible, a project should be designed to functionally integrate with adjacent properties by providing for reciprocal access easements, common drives, and common perimeter landscape planters.



Typical Industrial Site Layout

3. Lot assembly is encouraged as it provides greater opportunity to create efficient master planned projects in conformance with the intent of the design guidelines.
4. Industrial buildings should have a positive street presence and contribute to an attractive street scene by orienting buildings toward the primary street frontage. Public entrances and administrative/office areas should front the street. Primary entries should be clearly distinguished from secondary and service entries. Projects with few employees should attempt to place entries and the most active areas near the street to avoid long, “unguarded” walkways.
5. Entry and edge design features such as landscaping, architectural signage and monumentation, and enhanced paving should be incorporated in the project. Special materials, color, detailing, or equivalent architectural treatment should be incorporated into the building design at major entries.
6. Buildings shall be sited so as to screen loading and storage areas from public view. Where industrial uses are adjacent to non-industrial uses, appropriate buffering techniques such as increased setbacks, screening, and landscaping shall be provided to mitigate any negative effects of industrial operations.
7. Building entries should read as such, and be integrated with the overall building form. Doors should be designed at human scale. Variation in building height, wall plane, roof treatment, window placement, architectural detailing, etc. will define and emphasize public entries. Variation in material, texture, and/or color is also recommended as a means of identifying, building entries.

B. Setbacks

1. While respecting the minimum setbacks established by the Zoning Code, the front and streetside setbacks of new industrial development should generally approximate that of adjacent properties to establish a consistent image along the street. Some variation, however, should be provided in building and parking setbacks to avoid long monotonous building facades and provide visual interest.
2. Building setbacks should be proportionate to the scale of the structure. Larger structures require more setback area for a balance of scale and so as not to impose on neighboring uses.
3. Front and streetside setback areas shall be landscaped.
4. Building setbacks should be increased when adjacent to residentially zoned properties to mitigate negative impacts due to noise, vibration, light and glare, and aesthetics. Where an industrial project abuts a residentially zoned property, a minimum of 6 feet of the required

setback shall be devoted entirely to shrubs and trees, at least 6 feet in height (exclusive of any planter area curb).

5. Where the parking area of an industrial project abuts another industrially or commercially zoned property, a minimum 3-foot wide perimeter landscape buffer (exclusive of the planter area curb) is required. Where feasible to do so, integrate the landscape buffer with that of the adjacent property.

C. Environmental Considerations

1. Buildings should be designed and sited to maximize the use of sunlight and shade for energy savings, and respect the solar access of adjacent buildings. Orient buildings along east-west axis, where possible, to maximize passive solar effects. Place windows strategically to enable cooling by natural ventilation while also admitting daylight and enabling wintertime passive solar heating.
2. To the extent possible, site grading should relate to the natural surroundings and be designed to minimize grading by following the natural ground contours and recognizing existing drainage patterns. Graded slopes should be rounded to blend with existing terrain. Grading should emphasize and accentuate scenic vistas and natural landforms.
3. Large manufactured slopes should be avoided in favor of several smaller slopes integrated throughout the project. Smaller slopes are less obtrusive, more easily vegetated and can be used to add visual interest, preserve views and provide visual buffers where necessary.
4. Significant existing trees, vegetation and any other natural site attributes should be preserved to the greatest extent possible in the design and development of the industrial project. Site design that requires altering landforms and removing trees is discouraged.
5. In the mild Southern California climate, thermal mass (often in the form of concrete walls and tile floors) can mitigate fluctuations in indoor temperature, reducing the need for mechanical heating and cooling.

D. Vehicle Circulation and Access

1. Site access and internal circulation in industrial developments should promote safety, efficiency, and convenience. Vehicular traffic should be adequately separated from pedestrian circulation. Vehicular entrances should be clearly identified and easily accessible to minimize pedestrian/vehicle conflict.
2. Adequate areas for maneuvering, stacking and emergency vehicle access should be provided. Vehicular access, drives and circulation routes shall be designed so that all movements involved in loading, parking, or turning shall occur on-site, and not within the

public right-of-way. Internal circulation routes and parking areas should be separated. Continuous circulation should be provided throughout the site to the greatest extent possible to prevent awkward vehicular maneuvers. Dead-end driveways should be minimized. Vehicles should not be required to enter the street in order to move from one area to another on the same site.

3. The number of site access points or driveway aprons shall be minimized for aesthetic purposes, to achieve efficient and productive use of paved accessways, and to eliminate traffic hazards. They should be located as far as possible from street intersections (a minimum distance of 100 feet is recommended) and should be coordinated with existing or planned median openings and driveways on the opposite side of the roadway. Entrances and exits to and from parking and loading facilities should be clearly marked with appropriate directional signage where multiple access points are provided.
4. Design provisions, which allow for present or future reciprocal access with adjacent properties, are encouraged.
5. Where a property abuts an alleyway, service and vehicular access should be taken off of the alleyway. This is most strongly encouraged for development on narrow lots (less than 100' in width). Where these properties exclusively use the alleyway for service and vehicular access, a consistent and uninterrupted building frontage can be established for the entire length of the street. Alley improvements should coincide with site planning to minimize alleyway deterioration and address problems such as debris, safety, and any nuisance odors or hazards.

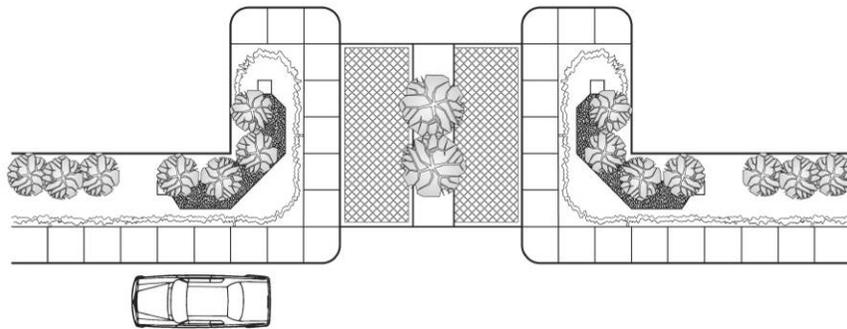
E. Parking

1. The industrial site should be a self-contained development capable of accommodating its own parking needs. The use of the public street for parking and staging of trucks is not allowed. In addition, parking is not allowed in the front and streetside setback areas.
2. Parking areas should be accessed from the street so that circulation to parking areas does not interfere with other site activities. Visitor parking should be located at the front and sides of buildings to be near primary building entrances.
3. Parking areas should not visually dominate the site. Large expansive paved areas located between the street and the building should be avoided in favor of smaller multiple lots separated by landscaping and buildings.
4. The visual impact of parking lots and structures shall be mitigated with landscaping. Parking lots adjacent to and visible from public streets must be adequately screened from view through the use of rolling earth berms, low screen walls, changes in elevation, landscaping or combinations thereof whenever possible. Landscaping materials should

have adequate room to grow and be protected from abuse by cars. Continuous concrete curbs shall be provided as wheel stops where parking adjoins landscaping.

F. Pedestrian Circulation

1. Industrial developments shall incorporate pedestrian walkways into site design to provide pedestrian connections from building entries to public sidewalks, plazas, and parking areas, and to buffer pedestrians from vehicular movement. Project entries and driveway areas should contain design features, including landscaping and textured paving, to break up the expanse of paving in a project. Paving materials should complement the architectural design. The use of stamped concrete, stone, brick, pavers, exposed aggregate or color concrete is encouraged.
2. Pedestrian walkways should be a minimum of 4 feet in width. Pedestrian walkways should be safe and clearly identifiable using varied surfaces, decorative paving, and landscaping to minimize pedestrian/vehicle conflict. At a minimum, varied surfaces should be used to delineate crossings at circulation drives and parking aisles.



Use decorative paving and landscaping to facilitate vehicular and pedestrian access at project entries.

G. Plazas and Open Space

1. Plazas and similar open space features are strongly encouraged as a site amenity and design detail. Buildings should be arranged to include opportunities for plazas, patios, open space areas, and employee gathering spaces with amenities such as outdoor seating, landscaping, water elements, pergolas, special lighting and other “place-making” features. These outdoor spaces should be functional and pleasant and should not appear as “left-over” spaces.
2. Plazas are encouraged where high levels of pedestrian activity are expected, such as adjacent to major entrances and food services, or between building clusters in an industrial/business park development.

3. Building entries and windows should look onto plazas and open space areas to enhance activity and security.
4. Outdoor employee break areas and lunch areas should be located away from loading areas or other high-traffic areas.

H. Fences and Walls

1. Walls and fences serve a major function in the industrial landscape and are used to screen vehicles, loading and storage areas, and utility structures. However, if not required for a specific screening or security purpose, they should not be utilized. The intent is to keep the walls as low as possible while performing their screening and security functions. The maximum height of walls and fences on industrial properties is 8 feet, pursuant to the Zoning Code.
2. Walls and fences should be planned and designed as integral parts of industrial development, and should be consistent with the landscaping and building design.
3. If street fencing is necessary, decorative types of view fencing, such as wrought iron, are encouraged. Solid fencing, such as stucco or masonry, is strongly discouraged when they will block the view of the buildings or provide hiding places.
4. Perimeter walls or fencing that do not front a public street should be of decorative masonry (split-face block, plaster/stucco finish), decorative metal (wrought iron), wood, hedges, or a combination of materials. They should be designed in a style, material and color to complement the development. Both sides of walls should be architecturally treated.
5. Tiered planting should be provided adjacent to project perimeter walls along street frontages to soften their appearance.
6. Walls should be eliminated or sited to provide additional setback areas at project entries to accommodate landscaping, signage, or street furniture.
7. Wall sections greater than 80 feet in length fronting a street shall incorporate at least two of the following design features, in proportion to the length of the wall:
 - A minimum 2-ft. change in horizontal plane for at least 10 feet.
 - A minimum 18-inch change in height for at least 10 feet.
 - A minimum 18-inch high raised planter for at least half the length of the wall.
 - Use of pilasters at 25-foot maximum intervals and at changes in wall planes.
8. Gates or comparable design solutions should be provided in perimeter walls or fences to allow emergency access and facilitate convenient pedestrian access.

9. Walls should be curved or angled at corner locations along street frontages to allow sight line views around the corner.

I. Exterior Lighting

1. Exterior lighting shall be used to provide illumination for the security and safety of on-site areas such as building entrances, parking, loading, shipping and receiving, walkways, and working areas. The design of light fixtures and their structural support shall be architecturally compatible with main buildings on-site.
2. Exterior lighting should be adequate but not overly bright. It shall be located and designed to avoid direct glare onto adjacent properties.
3. Buildings and landscaping can be illuminated indirectly to create a strong positive image. Concealing light features within buildings and landscaping can highlight attractive features and avoid intrusion into neighboring properties.
4. Photosensitive off/on switches are strongly encouraged for energy conservation and safety. Use energy-efficient ENERGY STAR® certified lighting fixtures and equipment when possible. Use energy-efficient means of lighting, including light sensors, low-voltage lighting, fiber-optics and solar lighting where applicable.

J. Freeway Frontage Properties

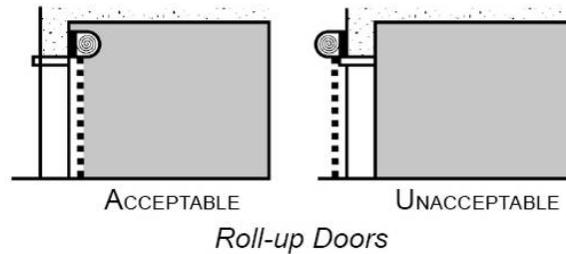
1. Site design of industrial developments should emphasize favorable views from the freeway. Avoid designing a project which “turns its back” to the freeway and offers views of blank walls, loading areas, and storage and service areas. Storage, loading and service areas should be screened from the freeway.
2. Significant landscaped areas shall be visible from the freeway. A minimum 5-foot wide landscape buffer shall be provided where a property abuts the freeway right-of-way.

K. Loading Areas

1. Loading areas shall be designed to prevent interference with vehicular circulation and parking, and to provide an unobstructed area for trucks to maneuver when accessing loading spaces.
2. Loading areas shall be located away from main customer entrances and the street, preferably toward the rear of the property.
3. Overhead (roll-up) doors shall not be directly open to public view, and shall be substantially screened from the street, the freeway, and residentially zoned properties. Screening may be

accomplished in a variety of ways, including the use of wing walls, the recessing of overhead doors (building articulation), landscaping, or a combination of these techniques. Fixed hardware for roll up doors shall be located on the inside of buildings to minimize visual clutter.

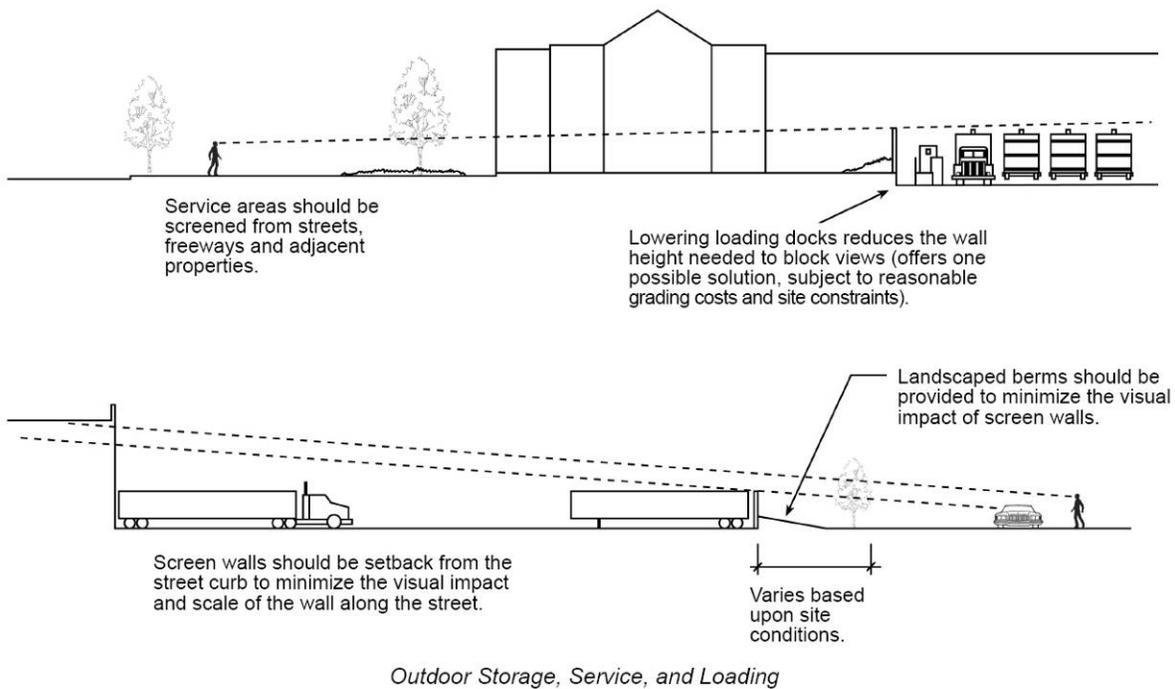
4. If located adjacent to residential areas, the design of overhead doors should minimize noise through devices such as rubber seals and/or other dampening features.



5. The grade of loading docks should be as low as feasible to minimize views from the street and the need for tall walls or fencing. Building segments above loading doors visible from the street and surrounding properties should conform with other guidelines pertaining to building features, materials and finishes.

L. Outdoor Storage and Service Areas

1. Outdoor storage and service areas (including, but not limited to, service entrances, loading docks and bays, outdoor storage of commercial vehicles) should be clearly defined and designated for convenient access. They shall not conflict with vehicular access, on-site parking facilities, pedestrian walkways, and customer entrances.
2. Outdoor storage and service areas should be located to the rear of a property so as not to face a public street. They shall not be open to view from the street, freeway, or residentially zoned properties.
3. Outdoor storage and service areas shall be screened from on-site and off-site public view with a combination of building features, decorative walls, and landscaping consistent with the architectural style and design of the building.
4. Outdoor storage and service areas shall be located so as to minimize negative impacts (visual, noise, dust, vibration, etc.) upon any neighboring residentially zoned properties.



M. Refuse and Recyclable Collection Facilities

1. Refuse and recyclable collection facilities shall be located so that there will be minimal intrusion (i.e. impacts associated with site views and odors) upon neighboring residentially zoned properties.
2. Refuse and recyclable collection facilities should be located for convenient access. Where the Zoning Code requires a number of trash bins for a given project, disperse the location of trash facilities for more convenient waste disposal by individual trash generators.
3. The location of refuse and recyclable collection facilities should be coordinated with the location of loading/service areas, and not readily visible to public view.
4. Decorative treatment of trash and storage enclosures shall be used to minimize the adverse visual impact of these areas. Trash disposal areas, including dumpsters, shall be screened from view by a 6-foot high enclosure with gates. Trash and storage enclosures shall be architecturally compatible with the project design, and landscaping shall be incorporated into their design to screen them and deter graffiti. Screening materials shall consist of fences, landscaping, and/or berming, and the use of natural terrain where possible.

N. Utility and Mechanical Equipment

1. All utility and mechanical equipment (wall-mounted meters, air conditioners, etc.) shall be screened from public view. This includes all ground, wall, and roof mounted equipment. Screening elements shall be an integral part of the building; no screening method shall give the appearance of being “tacked on.”

2. Where possible, integrate rooftop equipment into the overall mass of a building. At a minimum, roof mounted equipment shall be screened through the use of parapets, screening walls, equipment wells, mechanical room enclosures and similar design features. Screening devices other than parapet walls shall be designed as an integral element of the building massing. Picket fencing, chain-link fencing and metal boxes shall be avoided. The top of screens should be at least as high as the top of the equipment, with additional height provided where larger equipment units could be used in the future.



Exposed roof-mounted equipment is prohibited.

3. Typical ground-mounted equipment (such as transformers and heating units) shall be adequately screened with walls and/or landscaping. Large structures and/or equipment should be screened by the building from view of adjacent streets, freeway and properties.

4. All vents, gutters and downspouts, louvers, exposed flashing, etc. should be treated as design elements and be compatible with the rest of the building, or hidden from public view.

III. ARCHITECTURAL DESIGN GUIDELINES

While there is no mandated architectural style required for industrial structures in Baldwin Park, each project should possess an identifiable architectural theme and be of high quality design and materials. Industrial buildings should display unique, visually attractive qualities while having a unified composition. Multi-building projects should also use a consistent architectural style.

Industrial projects should give neighboring development a sense of unity through consistent building scale and massing. Yet, visual interest should be created with the use of a variety of architectural styles and individual building details to avoid monotonous industrial neighborhoods and enliven the public’s experience of the building. New projects should meet

or exceed the standards of quality which have been set by surrounding development and contribute to the improvement of the area.

A. Scale and Mass

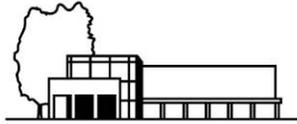
1. The scale and mass of a new industrial development should be consistent with neighboring developments and not overwhelm them with disproportionate size or a design that is out of character.
2. A single, dominant building mass should be avoided by using variations in massing and building form.
3. As appropriate to the function of a building, a combination of major and minor changes in building form should be incorporated to create visual interest and establish a transition to neighboring developments. Changes in building form should be used to emphasize office space and reception areas within industrial projects, emphasize public entrances and de-emphasize service areas, and define and shelter pedestrian walks and exterior spaces.
4. Primary building entries should be highlighted through the massing of the building. Greater height can be used to highlight and accentuate entries in the form of corner tower elements, tall voids, or a central mass meeting an entry plaza. Conversely, smaller building masses can also communicate the location of entries.
5. Typically, horizontal masses for building elevations less than 700 lineal feet shall not exceed a height to width ratio of 1:4 without a substantial architectural element that projects up or away from the building, such as towers, bays, lattices, or other architectural features. Buildings greater than 700 lineal feet shall not exceed a height to width ratio of 1:5 without massing variations. The extent of massing breaks and building projections should relate visually to the overall scale of the building.



The scale of industrial buildings can be reduced with windows that face the street, variation in massing at the primary entrance, and landscaping to soften the appearance.



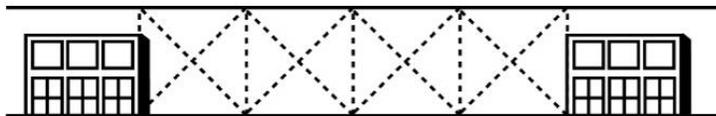
Avoid a single dominant building mass.



Highlight the primary entries through building massing and landscape treatments.

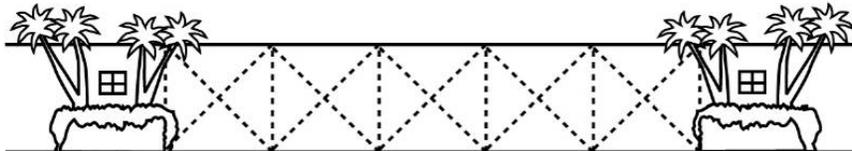
B. Building Articulation and Detailing

1. Building articulation and detailing should be used to create an interesting and individual design, diminish the massing of large structures, and be compatible with the scale of surrounding development. Building design shall avoid large monotonous facades, long straight-line building fronts, plain box shapes, and barren exterior treatment.



1:4 RATIO FOR BUILDINGS LESS THAN 700 LINEAL FEET

Provide a substantial architectural element, such as a tower, bay, lattice, planter box, or other feature, when the horizontal mass exceeds a height to width ratio of 1:4.



1:5 RATIO FOR BUILDINGS GREATER THAN 700 LINEAL FEET

Buildings greater than 700 lineal feet shall not exceed a height to width ratio of 1:5 without massing variations.

2. All elevations should be architecturally treated, however, facades visible from the freeway and major street corridors should be especially attractive and shall be fully articulated, and incorporate the chosen architectural theme in a consistent manner.
3. Articulation should include change of wall plane, door and window treatment, facade details, and other appropriate architectural treatment. A combination of compatible treatments should be used to create interest and variety, with attention given to treating particular architectural features in a balanced, yet uniquely detailed and decorative manner.
4. The staggering of planes along an exterior wall elevation creates pockets of light and shadow, providing relief from monotonous, uninterrupted expanses of wall. Wall planes should not run in one continuous direction for more than 60 feet without an offset.



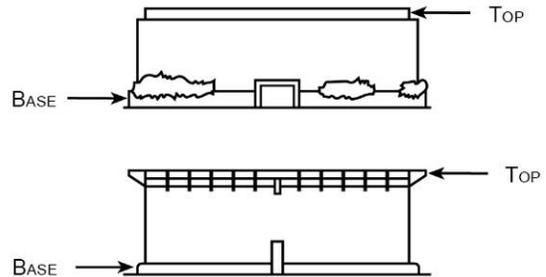
Avoid blank facades and barren exterior treatment.

5. Facades having a recognizable “base” and “top” are encouraged. The base should visually relate to the proportion and scale of the building. Techniques for establishing a base may include richly textured materials (e.g. tile or masonry treatments), darker colored materials, mullion, panels, reveals and/or enriched landscaping. Tops take advantage of the visual prominence of a building’s silhouette. Techniques for clearly expressing a top may include cornice treatments, roof overhangs with brackets, richly textured materials (e.g. tile, masonry or fluted concrete), and/or differently colored materials. Colored “stripes” are not acceptable as the only treatment.



Use building articulation, change of wall planes, door and window treatments, and other appropriate architectural detailing to create an interesting and individual design and diminish the mass of large industrial structures.

6. Buildings should incorporate architectural details and elements, which will reduce building scale at the street level, especially along pedestrian walkways. Awnings, canopies, arbors, trellises, etc. are effective in this regard. The appropriate use of other architectural details, including reveals, course lines, decorative cornice, columns, etc., is also encouraged as a means of creating interest, variety, and distinctive design. Details should reflect the structural and material integrity of the building; overly gratuitous ornamentation is discouraged.



A "top" and "base" should be established within the top-most and bottom-most one-eighth of a building.

C. Height and Roof Lines

1. The roof design should be considered as a component of the overall architectural design theme.
2. Roof forms should be simple, avoid a massive appearance, and reflect the internal organization of buildings.
3. Roof form and height should be varied to complement building mass and articulation. Vertical variations to the roofline should incorporate roof projections to avoid a false front/unfinished appearance.
4. The roofline at the top of the structure should not run in a continuous plane for more than 50 feet without offsetting or jogging the roof plane.



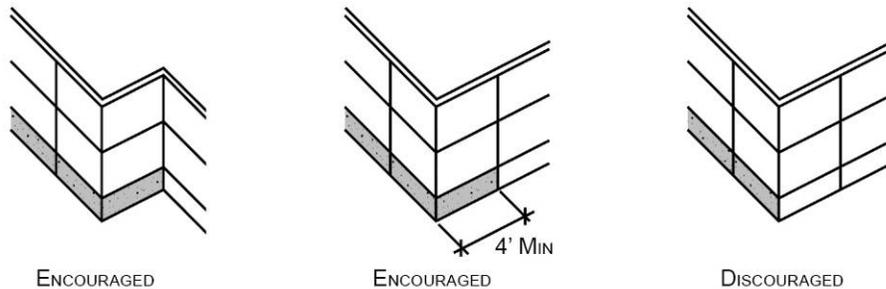
Varied roof forms that complement the building mass and articulation are encouraged.

D. Doors and Windows

1. Doors and windows are key elements of any structure's form, and should relate to the scale of the elevation on which they appear. Windows and doors can establish character by their rhythm and variety and help to provide depth and contrast on elevation planes. Windows and doors should be used to help mitigate building mass, establish scale, give expression to otherwise blank walls, and create a distinctive building design.
2. All doors and windows should be related with the chosen architectural style. Windows with widely varying styles are strongly discouraged. All doors and window frames should be composed of consistent material. Wherever possible, window sizes should be coordinated vertically and horizontally and window design should be consistent in terms of style and general arrangement on all building sides.
3. Window exposure should be maximized along pedestrian walkways. The use of opaque glass adjacent to pedestrian walkways is discouraged.
4. Window frames should appear substantial and should not be flush with the exterior finish. Windows should be designed to enhance building interest and articulation. Recessed windows or inset glazing are possible design considerations.
5. Windows located on the sides and rear of the project should also be consistent with the look and style on the front of the project.

E. Materials and Finishes

1. Materials and finishes should be suitable to the scale, character and design theme of the building and further lend variety and interest to the project.
2. The building and its elements should be unified by textures, colors and materials. Materials should be consistently applied and should be chosen to work harmoniously with adjacent materials. Piecemeal embellishment and frequent changes in materials should be avoided.
3. Buildings should be treated as a whole and finished appropriately on all sides to provide continuity. Materials tend to appear substantial and integral to the structure when material changes occur at changes in plane. Material changes not accompanied by changes in plane appear “tacked-on” and are strongly discouraged. Material changes, as well as changes in color, should not occur at external corners. Material and color changes may occur at “reverse” or interior corners or as a “return” at least 4 feet from external corners, with extended returns provided for large buildings.



Treatments for material and color changes at corners.

4. Exterior materials for industrial developments should be of high quality and low maintenance. Recommended materials include masonry, concrete, sandblasted concrete, textured block, brick, granite, marble, glass, painted metal elements and similar materials. Materials and detailing should have a substantial and long-lasting appearance. Metal siding should be avoided as the primary material, but may be used as an accent material if it is high quality and properly applied. Concrete blocks should also be avoided unless mitigated through careful and decorative design, texture and reveals.
5. Roofing materials should be durable. Where visible from the street, acceptable roofing materials include metal standing seam and concrete tile. Corrugated metal (standing rib metal roofs are permitted), highly reflective surfaces, and illuminated roofing and not permitted

6. Materials that will withstand abuse by vandals or accidental damage from machinery are strongly encouraged.
7. Accessory structures should be designed as an integral part of the project architecture and should be similar in material, color, and detail to the primary buildings.
8. The use of sustainable building materials is strongly encouraged. This includes using quality materials with a long life span, selecting materials that are not energy-intensive to manufacture, using building products made from recycled materials, and repairing and maintaining well-built existing structures to the fullest extent possible.

F. Color and Texture

1. For most architectural styles, the number of colors on the exterior should be limited to a maximum of three, with an additional contrasting color for accent. In general, the lighter colors should be used for the main body, with darker shades for trim and accent. The larger and simpler the building design, the more subtle the color should be to reduce the massiveness of large wall planes.
2. Off-whites, light grays, and muted earth tones are best suited and are appropriate for industrial developments. The use of strong or bright, unnatural colors, including the bright “white-on-white” color schemes. However, a greater variety of brighter, more intense colors are permitted to highlight architectural features such as awnings, canopies, doorways, window framing and trim, reveals, etc.
3. Color and finishes on exteriors of all elevations of a building should be coordinated to provide a total continuity of design. Unusual patterns and color schemes should be avoided. Garish, non-harmonious, or out-of-character colors should not be used.
4. The blending of compatible colors in a single facade or composition is a good way to add character and variety, while reducing, or breaking up the mass of a building. Lower wall wainscots and built-up or recessed reveals may be employed to add interest and break up vertical monotony.

G. Freeway Frontage Properties

1. Building design in industrial developments should address the freeway and visually enhance the freeway corridor. All facades visible from the freeway shall be articulated.
2. Large-scale forms and well-defined architectural elements will be most visible from the freeway and may prove appropriate.

IV. LANDSCAPE DESIGN GUIDELINES

Landscaping shall be an integral part of the site design for industrial developments. Landscaping enhances the aesthetics of industrial developments, breaks up the mass of industrial buildings, provides shading and climate control, and contributes to a pedestrian-friendly environment. It should also be used to provide a buffer between neighboring properties and incompatible land uses, screen service structures and loading areas, define building and parking area entrances, and define the edges of a project. When designed appropriately, landscaping acts as a unifying element within a project to obtain a cohesive appearance, to help achieve compatibility of a new project with its surroundings, and to enhance the overall industrial environment and streetscape.

These guidelines for landscaping for industrial developments are in addition to the landscaping standards set forth in the Zoning Code and the requirements of the Landscape Design Manual.

A. General Landscape Guidelines

1. Landscaping should be in scale with adjacent buildings and be of appropriate size at maturity to accomplish its intended goals.
2. Use of vines on walls is appropriate in industrial areas because such walls often tend to be large and blank.
3. Landscaping around the entire base of buildings is recommended to soften the edge between the parking lot and the structure. This should be accented at entrances to provide focus.
4. Trees should be located throughout the parking lot and not simply at the ends of parking aisles.
5. Landscaping should be protected from vehicular and pedestrian encroachment by raised planting surfaces, depressed walks, or the use of curbs.

B. Landscape Materials

1. All unpaved areas shall be landscaped with a variety of landscaping materials, textures, colors and forms including trees, shrubs, ground cover, flowering plants, boulders, rocks, walls, textured surfaces, trellises and other elements. Limited use of materials such as crushed rock, redwood chips, pebbles, and stones may be acceptable.
2. Plant species and hardscape materials should be selected based on their ability to complement the scale, mass, and color of the architecture. Create recognizable landscape patterns and themes.

3. Locate and choose planting types on their ability to create desired effects as follows:
 - a. Provide a backdrop and visual setting for the site's architectural elements.
 - b. Create focal points; highlight important architectural elements.
 - c. Frame and give prominence to building entrances.
 - e. Direct vehicular traffic; make an entry statement.
 - f. Direct pedestrian traffic; identify and shelter pedestrian walkways.
 - g. Provide a unified appearance along street frontages; reinforce the street hierarchy.
 - h. Protect sensitive uses from excessive solar exposure, glare, wind, noise, dust, odors, and undesirable views.

C. Preservation of Existing Trees

1. Where feasible, existing trees shall be maintained and cared for. The design and siting of buildings in an industrial development should take into account all established trees in order to avoid unnecessary removal. In addition, the root systems of established trees should be protected when siting a building and during construction.
2. Street trees are particularly important and should be considered before other factors in the design. Street trees provide residents with the beauty and comfort of shady streets and sidewalks and contribute to the overall character of the city. The removal of street trees is prohibited.

D. Sustainable Landscape/Hardscape

1. Permeable paving systems that allow rainwater to penetrate into the soil rather than running off into the street should be used. Stones and other materials found on site should be reused in the landscape design to conserve resources. Recycled content materials, salvaged materials, and sustainably harvested forest products are encouraged.
2. Native and drought tolerant plants are strongly encouraged. Because turf requires high water use, it should be used sparingly where possible. If turf is used, drought resistant varieties are strongly encouraged.
3. Water-efficient irrigation systems, such as low flow and drip equipment, shall be used. Rain sensors are required on all irrigation systems. When feasible, provide on site rainwater storage (such as capturing rainwater from the roof) to use in landscape irrigation.
4. Plants with similar watering needs should be grouped (on the same irrigation valve). High and low water use plants should be separated.
5. Refer to www.bewaterwise.com for additional recommendations for conserving water in landscape irrigation.

E. Landscape Lighting

1. Landscape lighting should complement and enhance the architecture and landscape of the development. Landscape lighting should be designed so that the light source is not visible. Lighting fixtures should be screened behind landscape features.
2. Landscapes should utilize discrete uplighting to illuminate planting and various landscape elements and to accentuate building facades and architectural details. Uplighting on walls of the structure help define space and create visual interest, while providing comfort and security.
3. Landscape illumination should be aesthetically pleasing and minimal. It should not flood the landscape with excessive light or spill into adjacent properties.